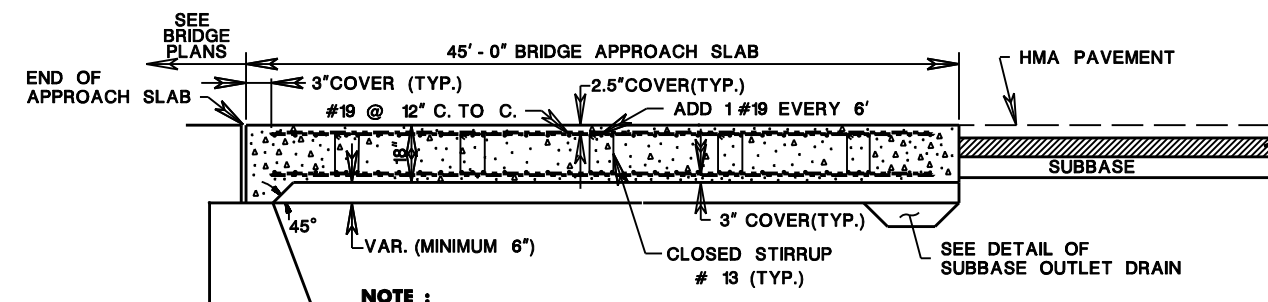
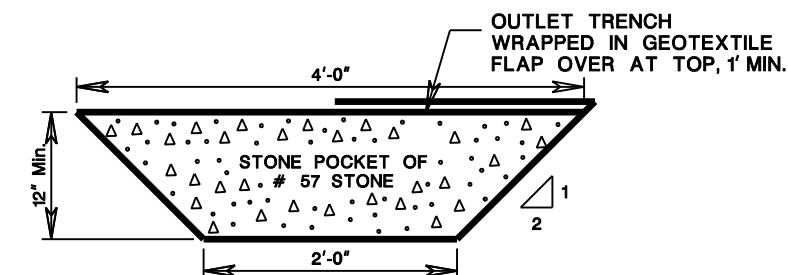


PLAN OF BRIDGE APPROACH SLABS



NOTE :
ALL LONGITUDINAL AND TRANSVERSE BARS
TO BE SECURELY WIRED TOGETHER.

SECTION A-A OF BRIDGE APPROACH SLABS

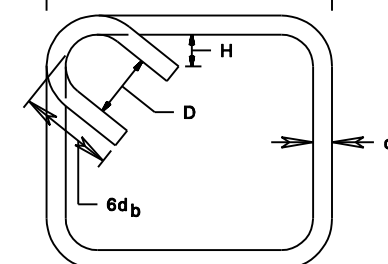


NOTES:

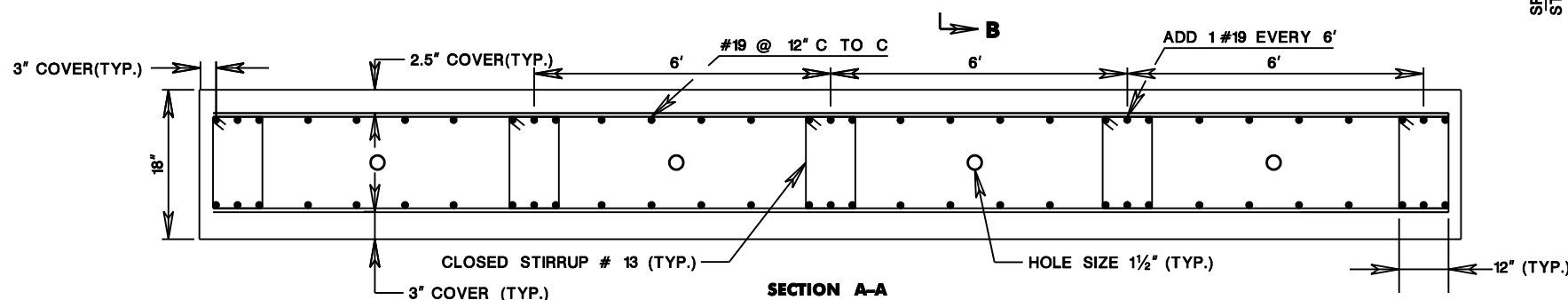
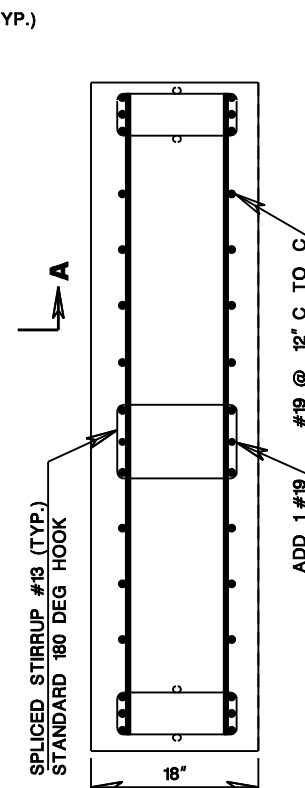
SUBBASE OUTLET DRAIN

- EXTRA BAR FOR EMBEDDED BEAM (EB) PATTERN TO BE SPACED NOT MORE THAN 6 FEET ON CENTERS.
- INSTALL REBAR CHAIRS TO SEPARATE TOP AND BOTTOM MATS (AS SHOWN).
- WIDER/LONGER SLABS MAY BE CONSTRUCTED PROVIDED THE C TO C SPACING OF EB EXTRA REBAR IS NOT MORE THAN 6 FEET.
- ALTERNATE DESIGN WITH STIRRUPS: CLOSED STIRRUPS SUBSTITUTED FOR REBAR CHAIRS. SPACING NOT MORE THAN 3 FEET C TO C. STIRRUP MUST ENCLOSE TOP 3 AND BOTTOM 3 BARS.
- ALL REINFORCEMENT TO BE CORROSION PROTECTED.
- CONSTRUCT APPROACH SLABS WITH CLASS A CONCRETE. HPC MAY BE USED FOR THE APPROACH SLAB CONSTRUCTION. APPROPRIATE PAY ITEM AS SPECIFIED IN THE STANDARD SPECIFICATION SHOULD BE USED. BASIS OF PAYMENT SHALL BE CUBIC YARDS.
- THE APPROACH SLAB SHALL BE SAWCUT GROOVED FINISHED. THE METHODS USED SHALL BE ACCORDING TO THE REQUIREMENTS SPECIFIED IN THE NJDOT STANDARD SPECIFICATIONS FOR CONCRETE DECK SLABS.
- BENDS IN STIRRUPS SHALL CONFORM TO CRSI STANDARDS.

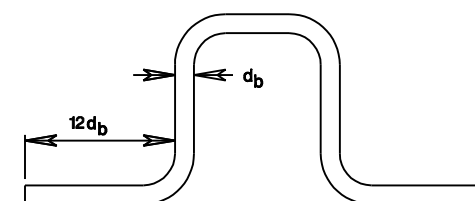
VARIES FOR SKEW
BRIDGE AT THE END



CLOSED STIRRUP # 13 (TYP.)



ALTERNATE # 13 STIRRUP SPLICED IN PAIRS



NEW JERSEY DEPARTMENT OF TRANSPORTATION

**BRIDGE APPROACH SLABS
ADJOINING
HMA PAVEMENT**